

About Natural AS

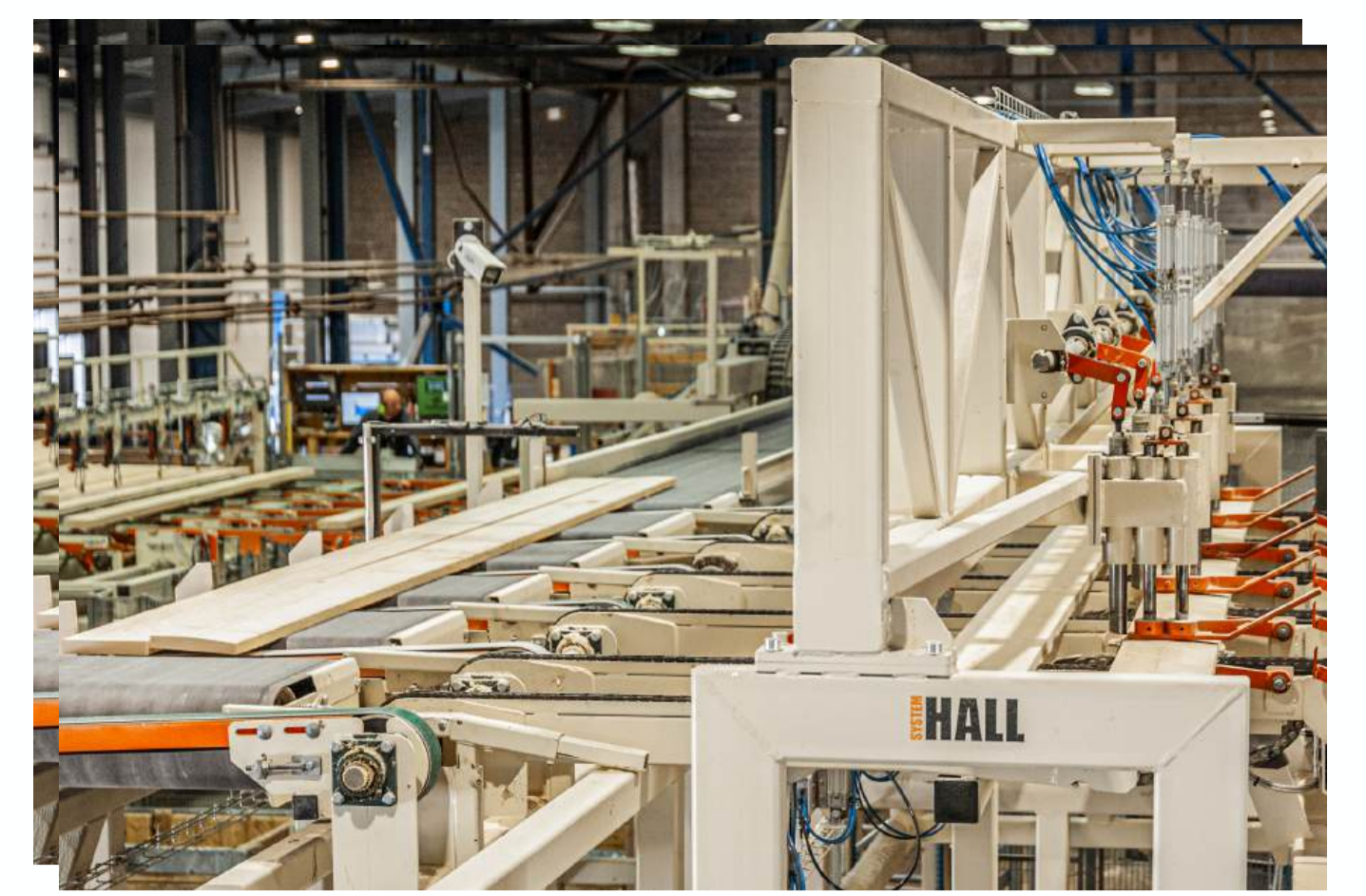
100% Estonian-owned company (founded in 1990) / Revenue: ~30 MEUR / Wood processed: 40% pine, 60% spruce / Employees: 70-80 / Annual planing volume: 75,000 m³ / 85% export / Location: Koeru, Järvamaa

Factories

Sawmill – since 1990 / Planing – since 2002 / Truss Factory – since 2016 / Finishing Factory – since 2020

Activities

Sorting / Drying / Planing / Strength Grading / Deep Impregnation / Precise Cutting (+/- 0.2 mm) / End Profiling / Truss Production / Industrial Finishing / Thermowood



GUIDE FOR HANDLING AND SECURING GOODS

1. General

2. Preparation for Loading

Carrier Responsibilities

Transport Vehicle Requirements

Cleanliness: The vehicle must be free from debris and dirt.

Waterproofing: The vehicle should be watertight to prevent moisture damage.

Flooring: Floors must be suitable for heavy loads, ensuring stability and protection.

Doors: Doors must close securely to protect the load during transit.

Odor: The vehicle should be free of any strong or harmful odors.

3. Loading

Loader Responsibilities

4. Load Securing Equipment and Attachment

Corners: Protective corner pieces should be used to avoid damage to the timber during transport.

Straps and Tighteners: Use appropriate straps and tensioners to secure the load effectively.

Securing: Ensure that all elements are tightly fastened to avoid shifting during transit.

Transporting Truss Structures: Special precautions are required when transporting trussed timber structures.

Carrier Responsibilities

The carrier must ensure that dispatched vehicles are equipped, maintained, and meet the following requirements:

- Vehicles must be approved by an authorized vehicle inspection agency.
- All maintenance and repair work must be conducted in an environmentally friendly manner and be well-documented.
- Tires must be in good condition, meet high environmental standards, and be suitable for the vehicle and road conditions, such as snow and ice.
- The vehicle body and attached parts must be intact, free from corrosion or brittleness, and capable of withstanding moderate pressure before breaking.
- Required emergency, securing, and fastening equipment must be present in all vehicles.
- Vehicles must always comply with the legal requirements of the country in which they are registered, as well as all transit country regulations.

The carrier is also committed to ensuring that:

- All vehicles meet at least Euro Class 5 standards or equivalent international standards.
- Drivers receive training in safe, fuel-efficient, and protective driving practices.

Drivers are aware of the additional risks associated with cargo shifting or parts of the load moving when handling the transport unit. Natural AS expects suppliers to provide driver training in compliance with Directive 2003/59/EC regarding initial qualification and periodic training for drivers. The loader must inspect the transport unit before moving the load. This check aims to ensure that the cargo is received undamaged and meets the Authorized Economic Operator (AEO) requirements for supply chain safety and security, confirming that the transport unit is not used for illegal activities such as smuggling. The inspection may be done visually or with tools. Any suspicious changes to the transport unit's structure must be reported to Natural.

1. General

The timber is transported in standard packages that comply with ISO 4472 or other relevant standards. When the wood is covered, such as with shrink-wrap or protective film, different friction values must be applied. Packages are secured with straps or wire, and strap integrity must be checked before loading. If straps are damaged or not firmly fastened, extra care should be taken to ensure the entire load is securely attached to the vehicle. However, steel or plastic straps should not be considered as load-securing elements.

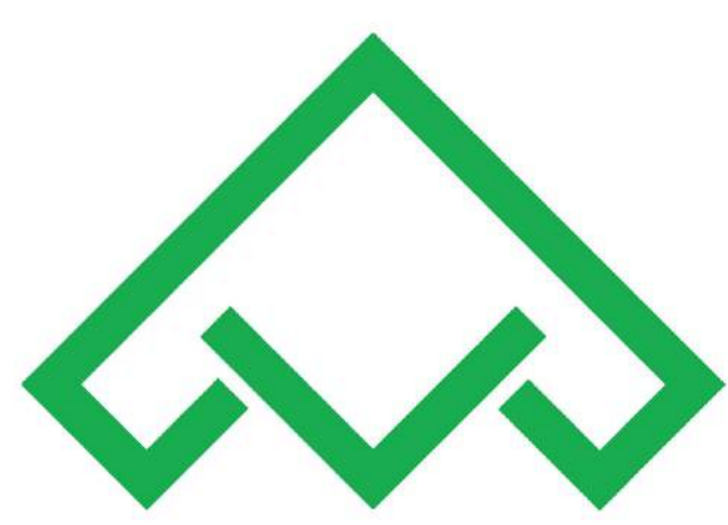
2. Preparation for Loading

The driver must ensure that the transport unit meets requirements and is ready for loading. This includes

positioning the vehicle and opening the cargo space so that the forklift operator has easy access to the transport unit for loading.

The driver must sweep the trailer and use approved securing accessories, such as corner protectors and load-securing straps.

Load planning must always consider the maximum permissible weight defined by each transit country. This also applies to overall weight distribution and maximum axle load. The legal load capacity is specified by each country and must always be adhered to.



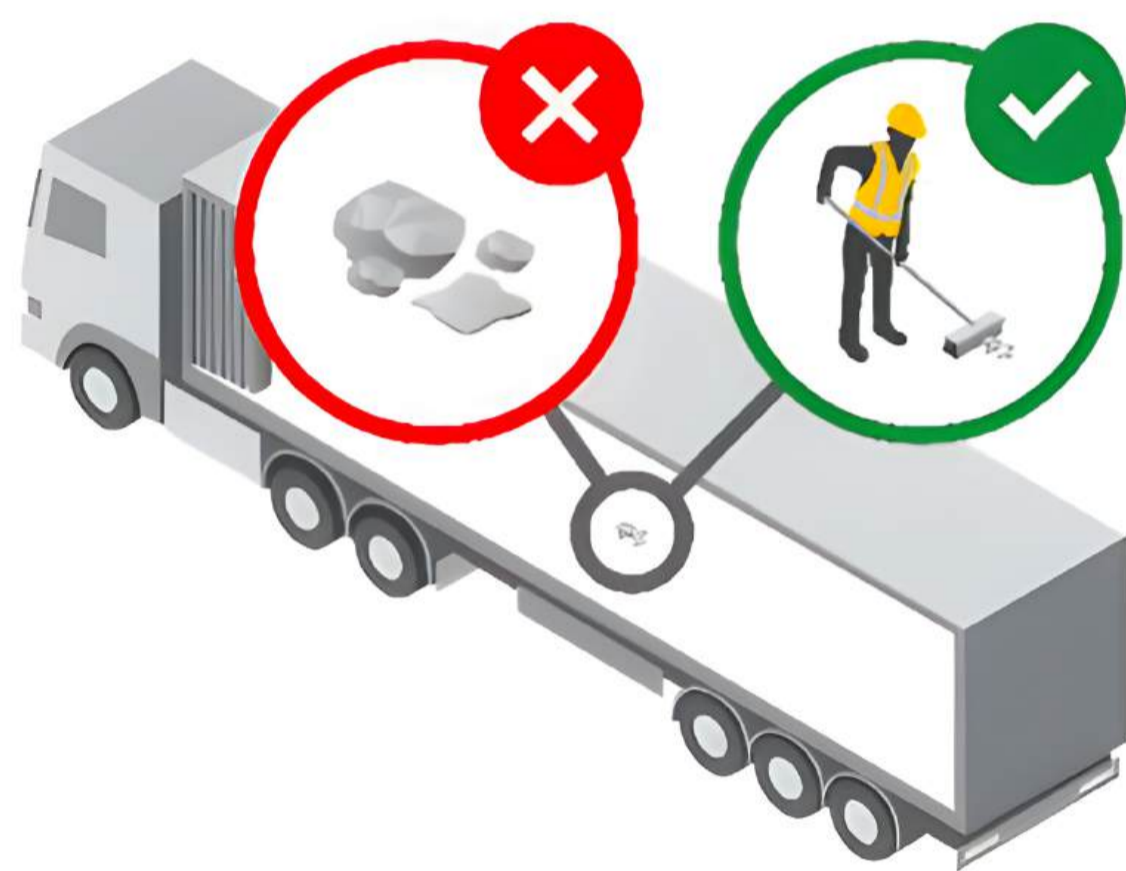
GUIDE FOR HANDLING AND SECURING GOODS

Transport Vehicle Requirements

Cleanliness

Before loading cargo, the transport unit must be completely clean. This means the trailer floor must be swept and cleared of debris prior to the loading operation.

It is essential that the cargo space be free of stones, dust, nails, remnants of previous loads, wood fragments, and any other items that could damage the cargo.

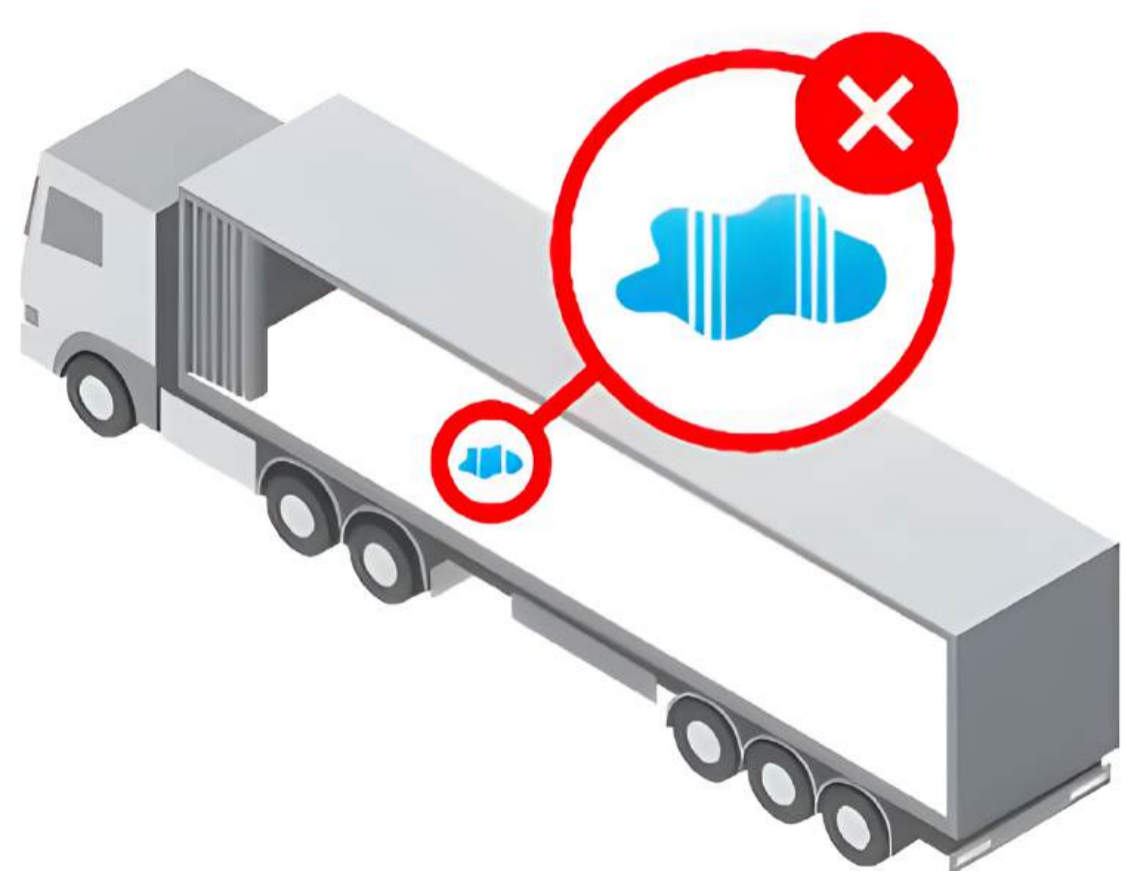


Clean the trailer floor!

Waterproofing

Natural requires that closed transport units be fully waterproof. The cargo space must remain dry at all times. Therefore, it is essential to inspect the roof, sides, curtains, doors, and floor for any damage that could allow water to enter the transport unit.

Any damage must be repaired professionally and durably before loading Natural AS goods. Moisture in the cargo space is unacceptable and may result in refusal to load.

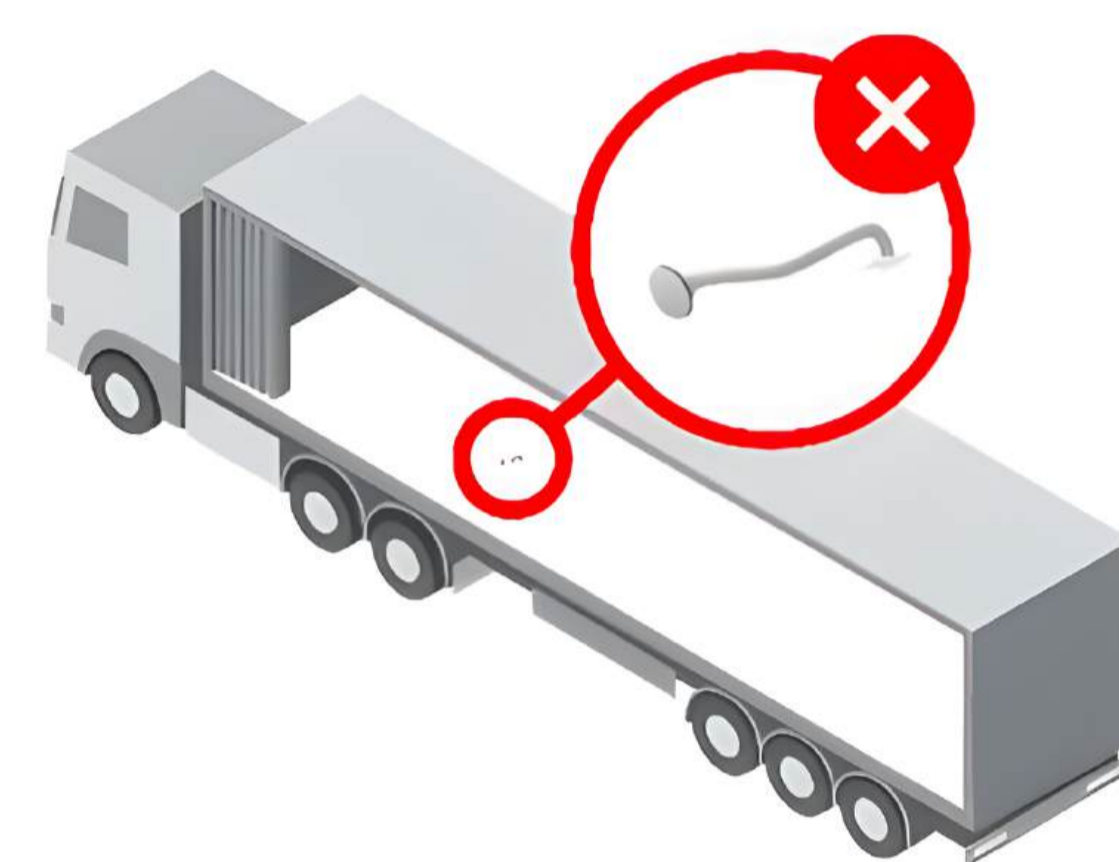


Ensure the cargo space is dry!

Floor

The floor of the transport unit must always be completely dry, clean, and smooth when loading or transporting Natural AS goods. All nails, stains (oil or grease), bolts, dust, remnants of previous loads, wood chips, protruding objects, etc., must be removed. Failure to meet these conditions may result in a refusal to load.

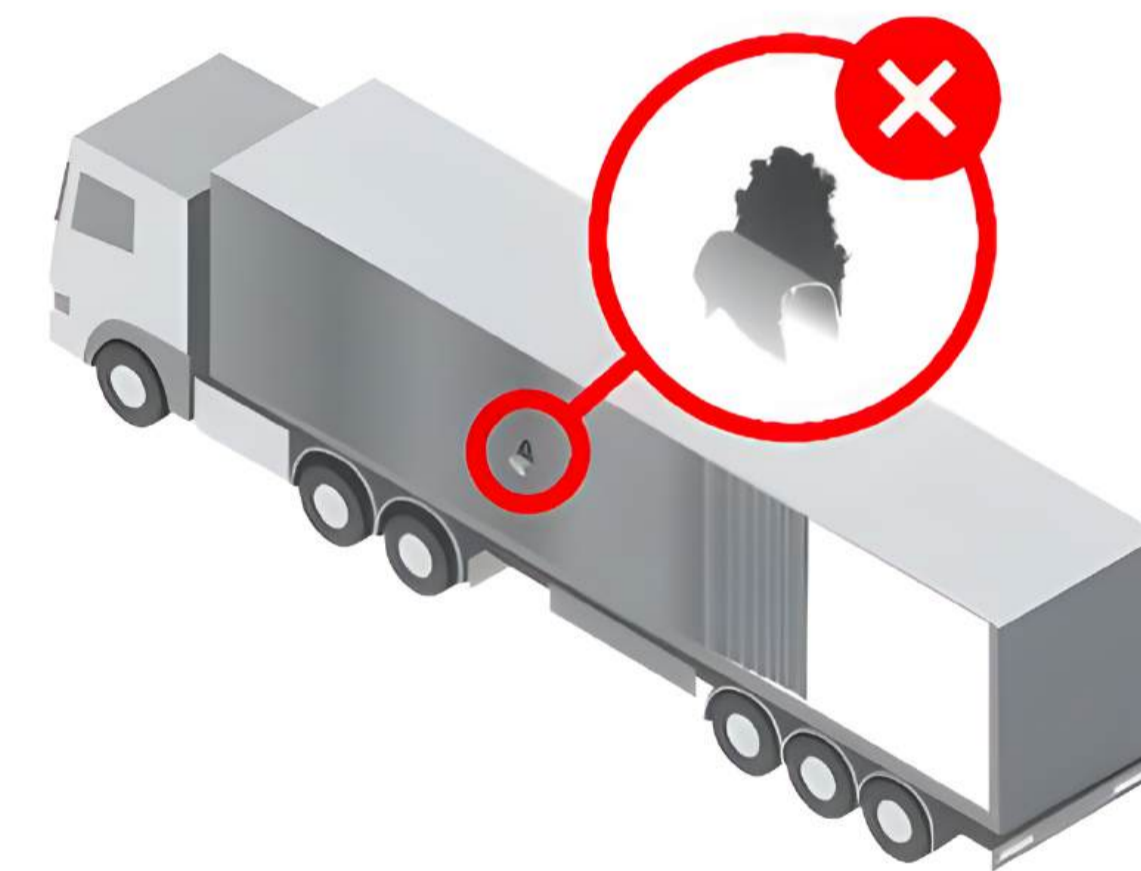
The floor must also be sufficiently strong (according to DIN EN 283 standard) to withstand and support loading and unloading equipment, such as forklifts.



Ensure there are no elements on the floor that could damage the new cargo.

Tarpaulin Covers

The sides and roof of the transport unit must be in good condition and fully watertight. Additionally, the sides should be even and free from any protruding objects. The top of the roof must be cleared of water, snow, or ice that could fall during loading. The trailer's front panel must be smooth and even to prevent damage. For curtain-sided trailers, the tarpaulin's opening and closing mechanisms must be fully operational, and the tarp itself must be free of any damage. All damage must be professionally and durably repaired before loading Natural AS cargo.



Inspect and repair the tarpaulin covers.

Doors

Doors, seals, and locks must all be in working order, fully watertight, and able to ensure the cargo space is completely sealed.

Odor

The transport unit is expected to be entirely free from any odors that could negatively affect Natural AS cargo. Strong smells from previous loads—such as tea, rubber, perfumes, leather, spices, fish, or chemicals—can easily transfer to the cargo.



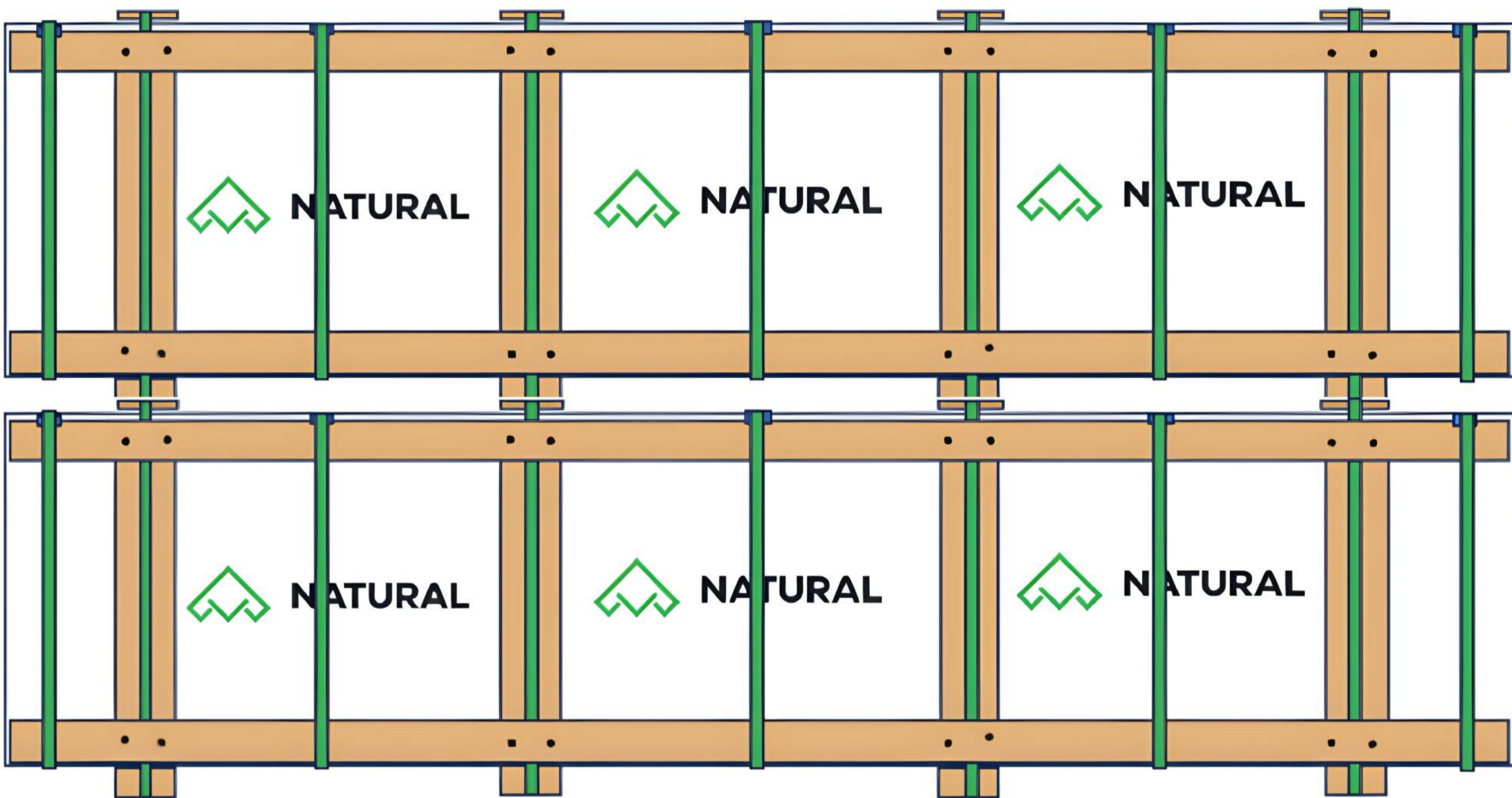
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3. Loading Loader Responsibilities

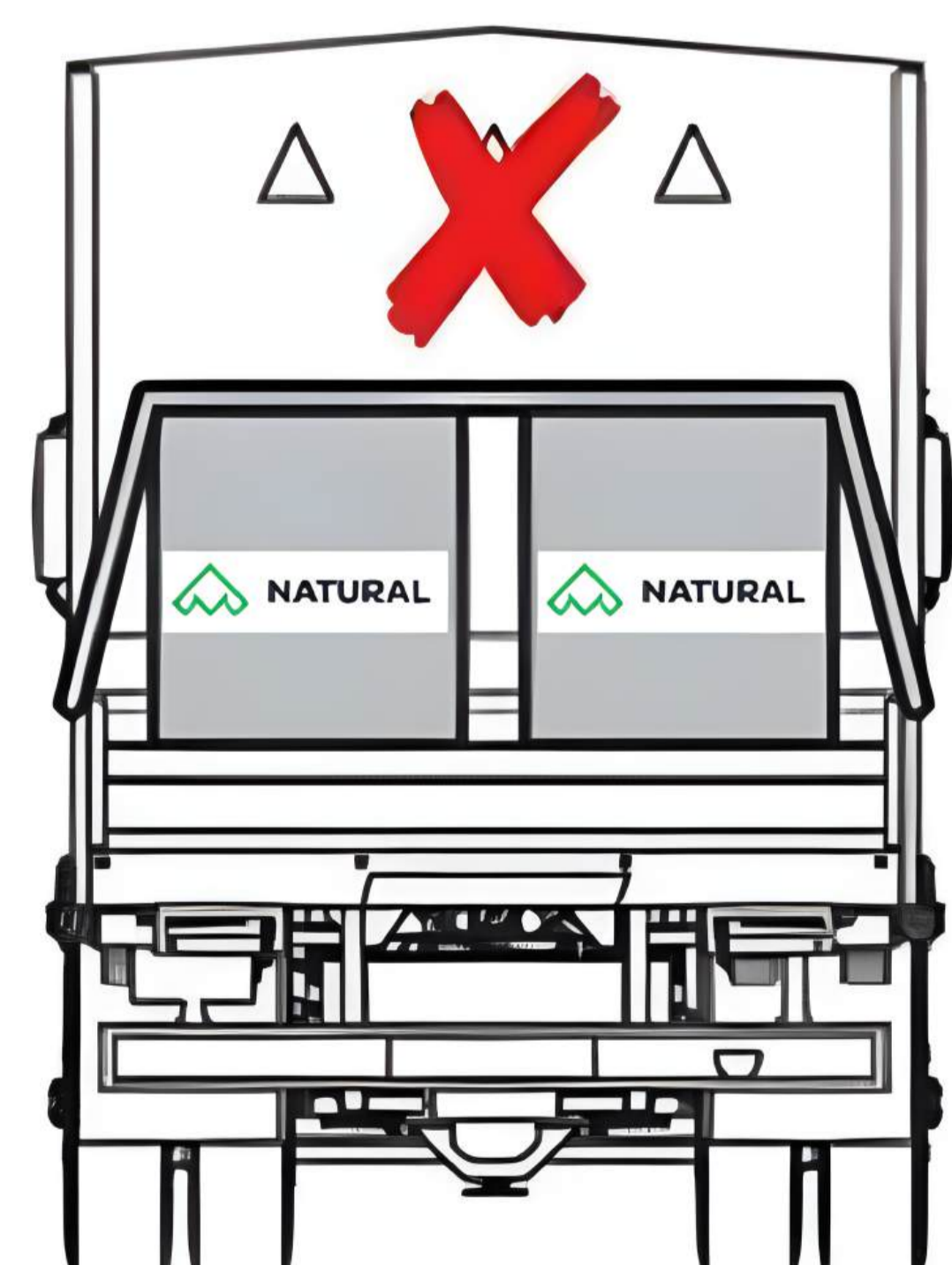
The loader must inspect the transport unit before moving the cargo. This inspection ensures the load is received undamaged and meets the Authorized Economic Operator (AEO) requirements for supply chain safety and security, confirming that the transport unit is not used for illegal activities, such as smuggling. Inspections may be performed visually or with tools. Any suspicious changes to the transport unit's structure must be reported to Natural.

During loading, the forklift forks must remain in a vertical position to avoid damaging the bottom, sides, or other parts of the packages during stacking and handling. The width and tilt angle of the forks should be adjusted to ensure stability and safety while handling.

Material packages must be handled safely and efficiently. When using forklifts, it is essential to consider the forklift's load capacity and fork length. Packages should be inspected before handling to ensure they are stable, undamaged, and in suitable condition, with no mechanical damage, broken



Timber packages should always be loaded lengthwise in the center of the trailer.

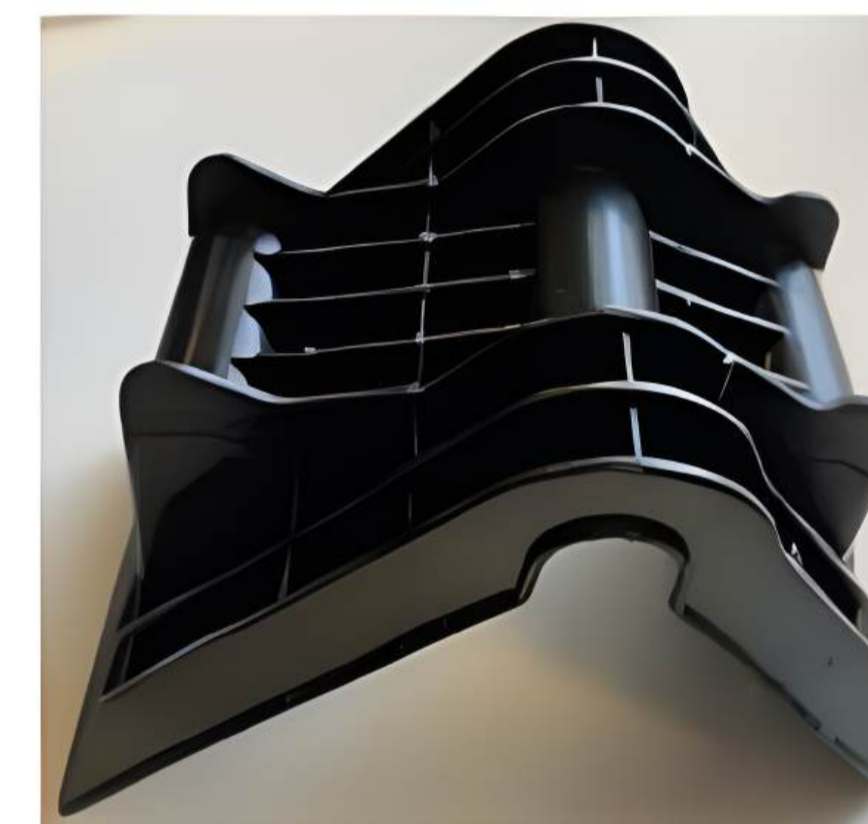
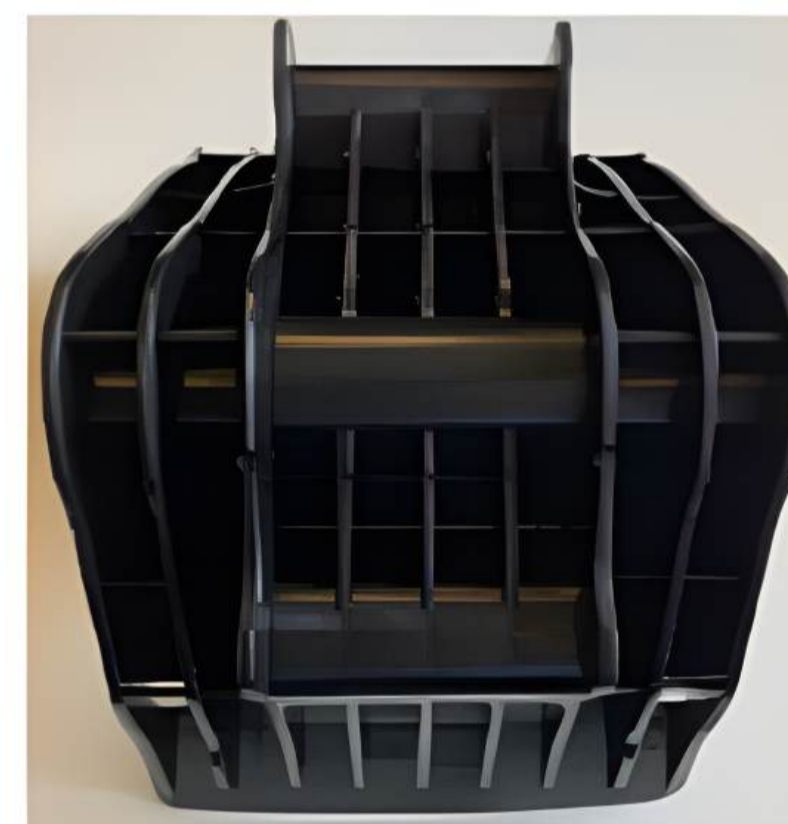


4. Load Securing Equipment and Fastening

All trailers must be equipped with adequate anchor points that comply with the applicable European standard, currently DIN EN 12640.

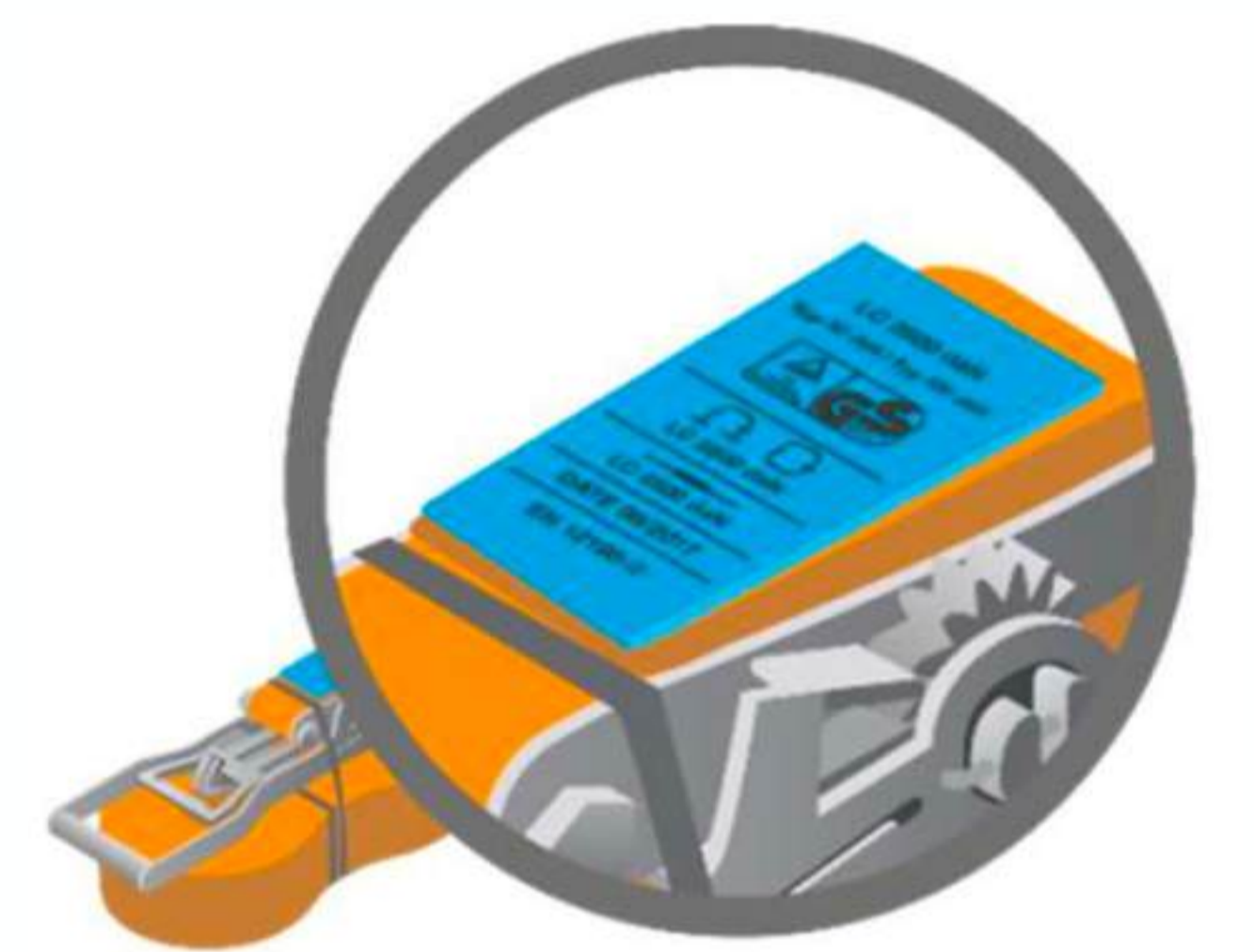
Corners

The supplier is responsible for ensuring that the correct quantity and type of edge protectors (corners), as specified in the transport agreement, are used. If the vehicle has damaged, incorrect, or insufficient edge protectors, the correct and undamaged protectors must be sourced. Edge protectors must be strong (not made of cardboard or weak plastic) and placed correctly to protect the cargo from damage.



Straps and Tensioners

The supplier must equip each transport unit with a sufficient number of straps and tensioners (in accordance with European standard EN 12195-2) suitable for securing the cargo being transported.



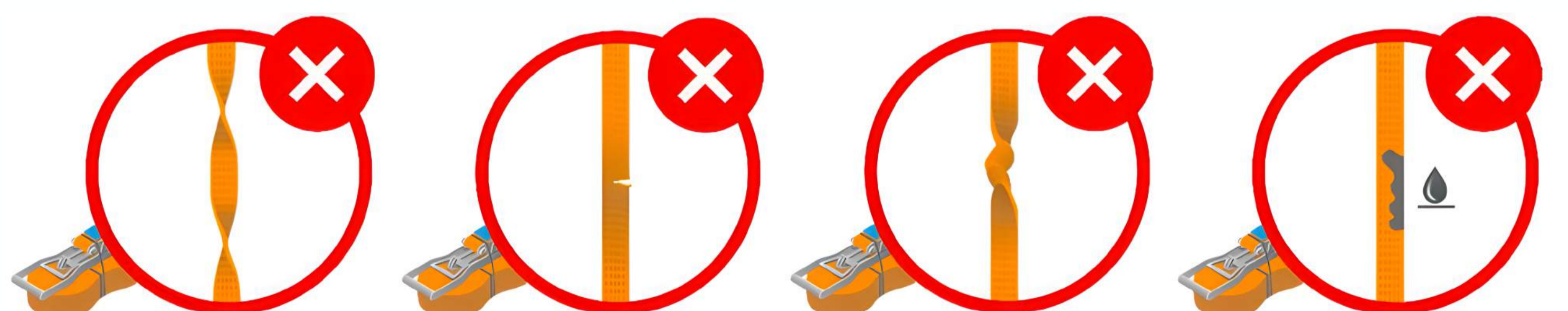
The load-securing strap and tensioner used must be separable. One-piece load-securing straps are not permitted for securing Natural AS cargo.

Load-securing strap and tensioner

The applicable STF (Standard Tension Force) on the tensioner side may be a maximum of 500 daN (1 daN ≈ 1 kg). Additionally, the strap or its labels must include relevant technical information. Straps should be placed straight and precisely centered on the edge protector.

European standard EN 12195-1:2010 applies to all European road transport carrying Natural AS goods. Domestic transport within Germany and Austria must also comply with VDI 2700.

Securing equipment must be in good condition and should always be inspected before loading. Any damaged or dirty equipment must be replaced.



The securing strap must not be twisted! The securing strap must not be damaged! The securing strap must not be knotted! The securing strap must not be dirty!

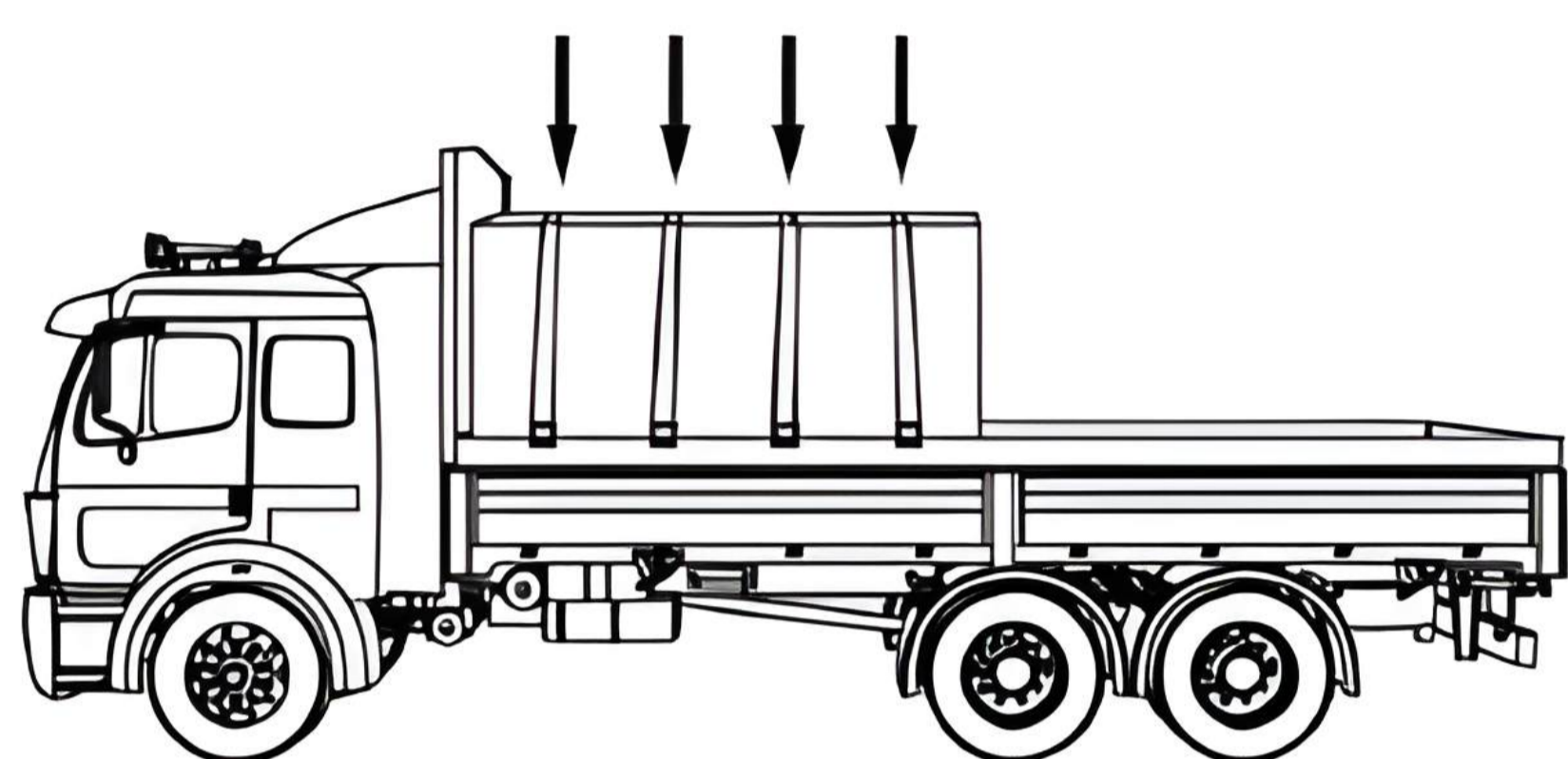


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Securing

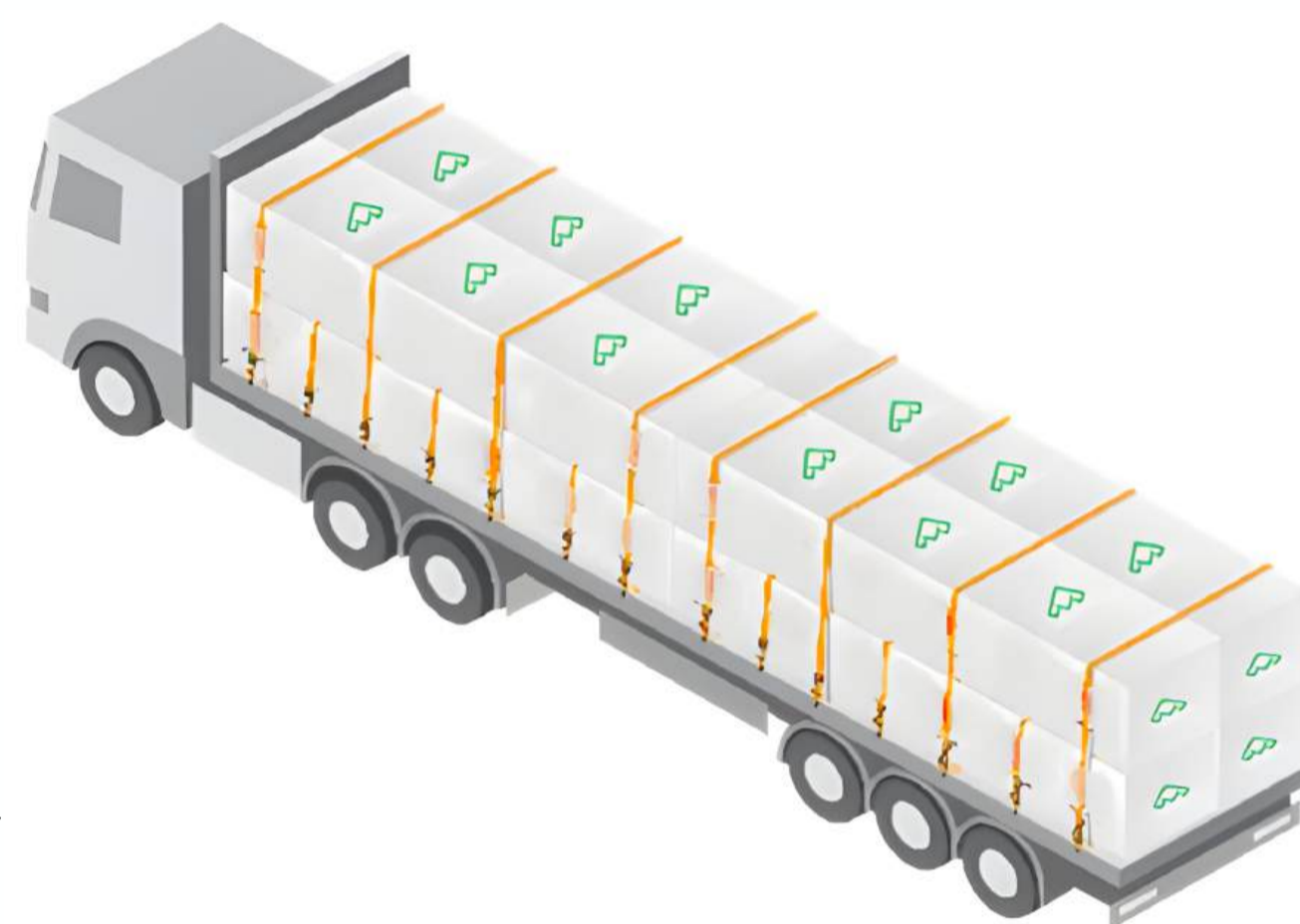
In general, units should be loaded against the front panel of the trailer. However, in some cases, leaving space may be necessary. In such situations, additional securing measures must be applied.

Cargo sliding and tipping must be prevented by securing with straps, blocking, or a combination of methods to withstand accelerations, decelerations, and centrifugal/side forces during transport. Securing should be done in a way that does not damage the cargo with the materials used for fastening.



If the load is not placed against the front panel and there is free space, additional securing is required.

Before the driver leaves the loading site, all responsible parties must verify that the load is secured in accordance with this guide. The driver must regularly check the load securing during the journey, especially after heavy braking or other unusual events. Load securing must also be inspected after any additional loading or unloading during the trip.



Timber packages must be secured from the top with edge protectors. At least one strap is required per meter of each timber package. A suitable number of base supports should be placed under the packages laterally (not lengthwise).

In some cases, when loading multiple layers, intermediate strapping along with top strapping may be necessary. If packages are not loaded against the front panel, additional forward securing is required.

During road transport, the trailer must not be left unattended to prevent unauthorized access to the transport unit and cargo. When the driver leaves the truck/trailer, the vehicle/cabin must always be locked. The route should be planned to allow the driver to take mandatory breaks and rest at secure, monitored locations.

Additional considerations:

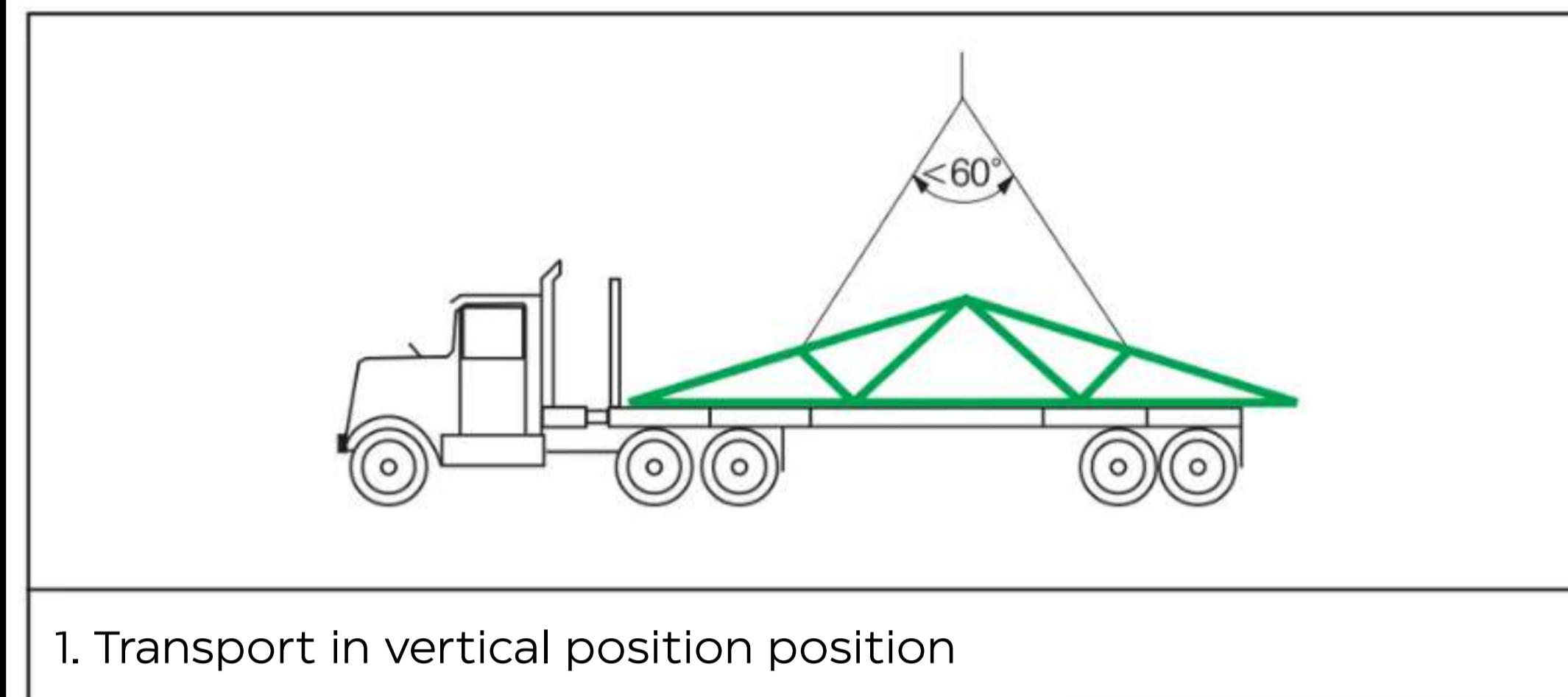
- Check that the crane's lifting capacity is sufficient for unloading the packages.
- Exercise caution when removing securing equipment (boards and tightening straps) as the load may shift.
- Secure the load with appropriate supports (e.g., angled supports).
- When removing nails, keep packages against a solid base.
- Be cautious when opening packages, as individual trusses can easily tip over.
- Avoid using steel slings, as they may damage the chords and/or joints/gang-nail plates of trusses. If steel slings must be used, apply protective padding at the sling contact points.

Transport of Gang-Nail Trusses

Gang-nail trusses are delivered in packages securely bound by the factory. The trusses should be secured with load-securing straps in accordance with the same principles as timber packages.

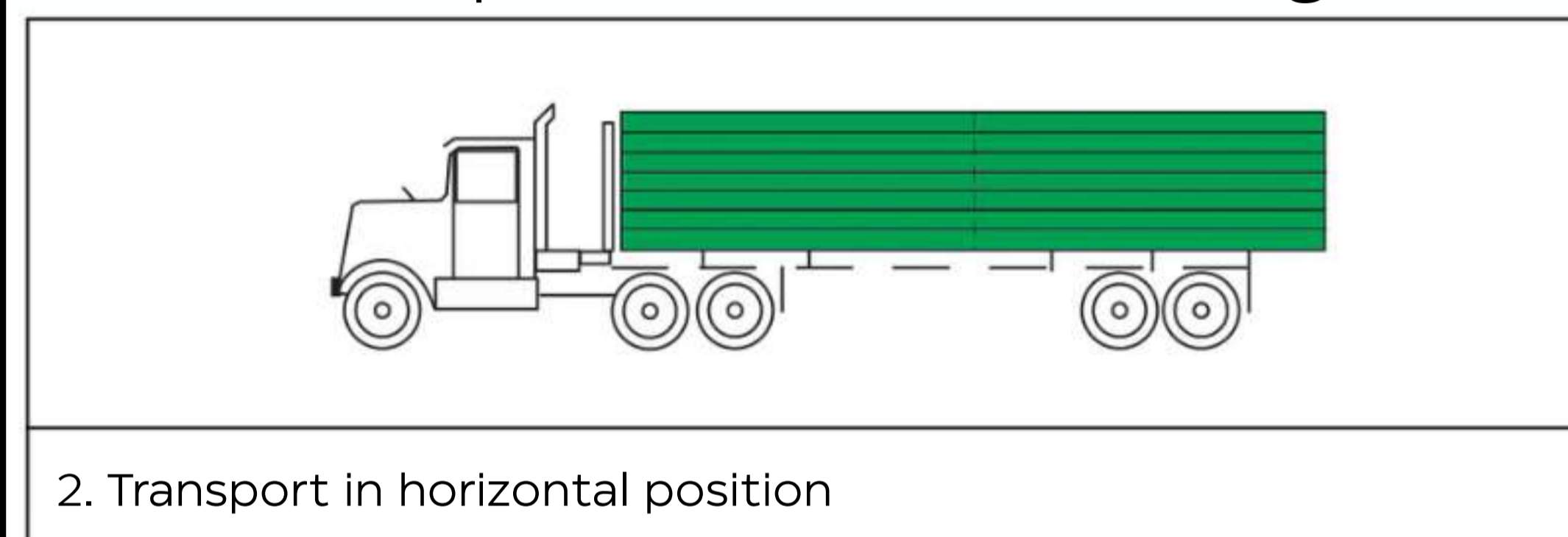
Trusses are transported in two positions:

- Vertical position, as shown in Figure 1.;



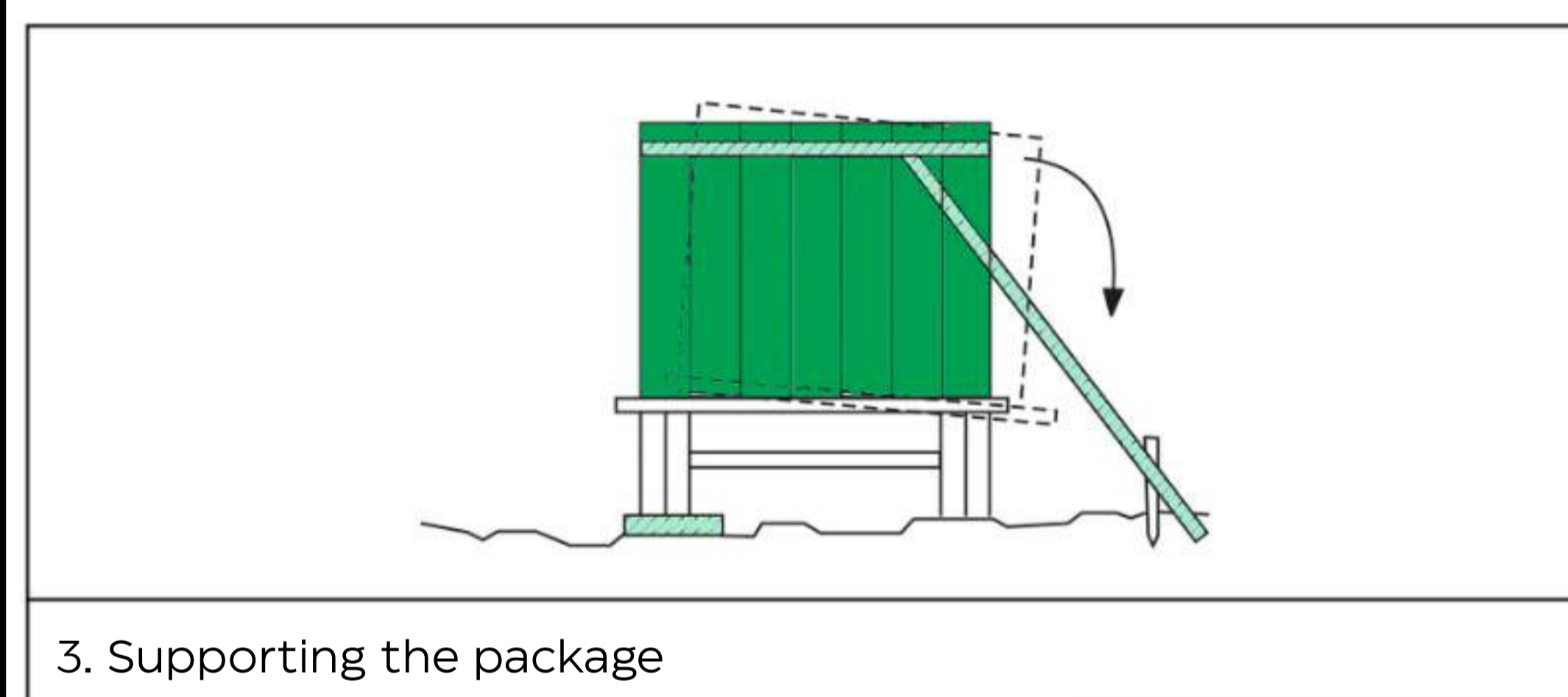
1. Transport in vertical position position

- Horizontal position, as shown in Figure 2.



2. Transport in horizontal position

If there is uncertainty about the exact weight of the packages, this should be confirmed with the manufacturer. Unloading should be carried out using a construction crane or truck crane (in exceptional cases, a forklift can be used, but only when the trusses are transported in a vertical position). Packages must be secured to prevent tipping, as shown in Figure 3.



3. Supporting the package

Key points to consider:

- Verify that the crane's lifting capacity is sufficient for unloading the packages.
- Exercise caution when removing securing elements (boards and tightening straps), as the load may tip over.
- Secure the load with appropriate supports (e.g., angled braces) to prevent shifting.
- When removing nails, keep the packages against a stable base.
- Be careful when opening packages, as individual trusses can easily tip over.
- Steel slings are not recommended, as they may damage the truss chords and/or joints/gang-nail plates. If steel slings must be used, apply protective padding at the sling contact points.
- It is recommended to use flexible lifting slings (straps). Lighter and shorter trusses may be unloaded manually.